

ORDINANCE 91-54

AN ORDINANCE OF THE CITY OF DELAND, FLORIDA CHANGING THE ZONING FROM R1-B, SINGLE FAMILY RESIDENTIAL DISTRICT, TO PD, PLANNED DEVELOPMENT DISTRICT, ON DESCRIBED PROPERTY LOCATED ON THE EAST SIDE OF SOUTH WOODLAND BOULEVARD BOUNDED BY EAST WINNEMISSETT AVENUE TO THE NORTH, ALABAMA AVENUE TO THE EAST AND VOLUSIA AVENUE TO THE SOUTH; APPROVING A DEVELOPMENT PLAN FOR THE FLORIDA DEPARTMENT OF TRANSPORTATION DISTRICT OFFICE COMPLEX, A PLANNED DEVELOPMENT CONSISTING OF APPROXIMATELY 8.29 ACRES OF PROPERTY LOCATED ON THE EAST SIDE OF SOUTH WOODLAND BOULEVARD BETWEEN EAST WINNEMISSETT AVENUE, ALABAMA AVENUE AND EAST VOLUSIA AVENUE; DIRECTING A CHANGE IN THE COMPREHENSIVE ZONING MAP; CONDITIONING FINAL DEVELOPMENT APPROVAL ON A DETERMINATION OF CAPACITY; PROVIDING FOR SEVERABILITY AND AN EFFECTIVE DATE.

WHEREAS, the Florida Department of Transportation (f/k/a the State Road Department and hereinafter referred to as "FDOT") owns an 8.29 acre parcel of land located on the east side of South Woodland Boulevard between East Winnemissett Avenue, Alabama Avenue and Volusia Avenue, which is more particularly described in the legal description attached hereto as Exhibit "A" and by this reference made a part hereof; and

WHEREAS, a portion of the land described in Exhibit "A" is currently subleased to the State Department of Highway Safety and Motor Vehicles; and

WHEREAS, FDOT has applied for a change of zoning from the present zoning classification of the parcel described in Exhibit "A" from R1-B, Single Family Residential District, to PD, Planned Development District; and

WHEREAS, said rezoning as a PD District is consistent with both the City of DeLand Comprehensive Plan and the purpose and intent of Section 33-24 of the Code of Ordinances of the City of DeLand (the "Code"); and

WHEREAS, FDOT has submitted an application for approval of a development plan for a district office complex to encompass all of the property described in Exhibit "A" (hereinafter "the property" or "the project"); and

WHEREAS, the proposed development plan meets or exceeds the minimum conditions and standards for a Planned Development District with respect to minimum development size, perimeter setbacks, comprehensive plan consistency, internal compatibility, external compatibility, intensity of development, open spaces, plazas and recreation, sidewalks, trails and bikeways, environmental constraints, internal

access and circulation, external transportation access, off-street parking, public facilities and unified control;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF DELAND, FLORIDA:

Section 1. The City Commission has held a public hearing on the application for a change of zoning from R1-B, Single Family Residential District, to PD, Planned Development District, for the land described in Exhibit "A."

Section 2. Pursuant to the determination made at public hearing, the City Commission hereby amends the Comprehensive Zoning Map of the City to include the land described in Exhibit "A" as PD, Planned Development District.

Section 3. The zoning of the property as PD, Planned Development District, is made pursuant to Section 33-24 of the Code and the development of this property shall take place in accordance with that Section and in substantial compliance with the Development Plan Map and Report submitted by FDOT which shall be filed and retained for public inspection in the office of the City Planning Department and constitute a supplement to the official Zoning Map of the City.

Section 4. The failure of the approved Development Plan to address a particular permit, condition, term or restriction shall not relieve FDOT of the necessity of complying with the law governing said permitting requirements, or other conditions, terms and restrictions. All land development regulations of the City are applicable to the PD District except to the extent that they conflict with a specific provision of the approved Development Plan set forth in Section 5 below.

Section 5. The approved Development Plan for the FDOT district office complex is set forth below:

(a) Development Concept: FDOT proposes to construct a 110,000 square foot addition to the existing district office building on site in order to consolidate functions and personnel presently housed in four separate locations throughout the City into one central office location. The Development Plan for the District Office Complex shall include all lands and buildings subleased to the State Department of Highway Safety and Motor Vehicles which share common access, parking and stormwater management facilities with the FDOT buildings. FDOT shall be responsible for securing the cooperation of and ensuing compliance by the State Department of Highway Safety and Motor Vehicle with the requirements of this Ordinance.

(b) Phases of Development: This project is projected to be developed over a three year period. The first phase includes the 110,000 square foot addition to the existing office building which shall not exceed a maximum height of 85 feet, exclusive of the elevator penthouse and parapet walls, if any. The second phase includes remodeling the existing office building. Specific conditions for permitting each phase shall be determined at the time of final site plan review.

(c) Land Use and Intensity: The location and approximate size of the land use areas are shown on the Development Plan Map and Report. The final site plan for the property shall be consistent with the limits described below:

<u>Existing-On Site</u>	<u>Footprint (Sq. Ft.)</u>	<u>Total Gross (Sq. Ft.)</u>	<u>No. of Employees</u>
District Office	11,715	23,200	92
Gas House	609	609	2
Location/Credit Union	1,760	1,760	6
Soil Lab	8,975	17,950	25
Elevator/HC Restroom	1,133	4,500	0
Trailers	5,280	5,280	37
Radio Transmitter	625	625	0
Sub-total	30,097	53,924	162
DMV	3,360	3,360	13
FHP	2,800	2,800	12
Total	36,257	60,084	187

Proposed-On Site

District Office	11,715	23,200	2
Gas House	609	609	2
Location/Credit Union	1,760	1,760	1
Soil Lab	8,975	17,950	25
Elevator/HC Restroom	1,133	4,500	0
Radio Transmitter	625	625	0
New Addition	34,035	110,000	424
Sub-total	58,852	158,644	454
DMV	6,160	6,160	16
FHP	0	0	0
Total	65,012	164,804	470

<u>Land Use</u>	<u>Existing Sq. Ft.</u>	<u>%</u>	<u>Proposed Sq. Ft.</u>	<u>%</u>
Bldg. Footprint's	36,257	10.0%	65,012	18.0%
Parking-Pervious		0.0%	55,063	15.2%
Parking-Impervious	82,765	22.9%	92,764	25.7%
Retention	20,523	5.7%	24,643	6.8%
Landscaping & Other-Pervious	169,067	46.8%	71,130	19.7%
FHP-Impervious	44,625	12.4%	44,625	12.4%
FHP-Pervious	<u>7,875</u>	<u>2.2%</u>	<u>7,875</u>	<u>2.2%</u>
Total	361,112	100.0%	361,112	100.0%
Total Pervious	197,465	55.0%	158,711	44.0%
Total Impervious*	163,647	45.0%	202,401	56.0%
Total Sq. Ft.	361,112		361,112	
Total Acres	8.29		8.29	

*Unless the City amends its Comprehensive Plan to allow for a greater percentage of impervious area, FDOT must decrease the amount of impervious areas shown on the final site plan to 50%. The 50% maximum impervious standard contained in this Ordinance shall supercede any subsequent Comprehensive amendment which decreases the allowable percentage of impervious area.

(d) Special Regulations: FDOT shall mandate compliance with this Ordinance and all applicable land development regulations in the design criteria package for the 110,000 square foot office addition and shall secure final site plan approval before it issues a Notice to Proceed with any work on site. The City shall not be responsible for any delay damages or extra costs incurred by FDOT on account of FDOT's failure to notify bidders of City requirements or to secure final site plan approval before awarding the design-build contract.

(e) Open Spaces, Plazas and Recreation Areas: At least 15% percent of the property shall be retained as open space and passive recreational areas.

(f) Environmental Considerations: FDOT shall:

- (1) retain at least 7 of the 11 specimen trees on site;
- (2) designate at least fifteen percent (15%) of the land area within the development for tree protection; and
- (3) plant a sufficient number of trees on the property to meet the minimum tree coverage requirements of the Tree Protection Ordinance. No clearing of trees or any other

development activity shall be permitted in designated tree protection areas except for the removal of trees damaged by fire, windstorm, lightning or other acts of God which pose imminent danger to life or property.

(g) Public Facilities: All necessary public facilities and services required to support development of the property shall be available concurrent with the impacts of development. If the necessary public facilities and services are not available concurrent with the impacts of development, FDOT may be required to bear the reasonable cost of providing same as a condition to final site plan approval. Potable water, sanitary sewer and solid waste service shall be provided by the City of DeLand upon payment of the usual and customary fees and approval of final engineering plans for each phase of the development.

(h) Stormwater Drainage: The development shall meet or exceed the requirements of the Stormwater Management and Conservation Ordinance, Chapter 25B of the Code, with respect to both existing conditions and any new development. FDOT shall correct existing drainage problems on site as part of the new construction and may be required to contribute a reasonable share of the City's cost of upgrading the stormwater management facilities at Earl Brown Park as a condition of final site plan approval.

(i) Roadways: FDOT estimates that the Development Plan will not adversely affect the level of service on area roadways because many employees participate in FDOT sponsored car/van pool programs and because much interoffice travel will be eliminated by consolidating District personnel in one central facility. Nevertheless, standard traffic projections indicate the need for further study since the number of employees is higher than previously estimated. Therefore, FDOT shall supplement the traffic impact analysis previously submitted to the City to review the actual impact of the proposed development on roadway segments within a one mile radius of the property. If the level of service on any roadway segment falls below the level of service standard adopted in the City's Comprehensive Plan, FDOT shall prepare and implement a traffic action mitigation plan to ensure 100% mitigation of the impacts of the proposed development. The traffic action mitigation plan shall include a schedule for implementing the required mitigation activities including, but not limited to the following: turn lanes, signalization, van/car pool programs, incentives to use available mass transit and staggered work hours.

(j) Off-street Parking. FDOT estimates a reduced need for on-site parking due to employee participation in car/van pool programs. Within six months after substantial completion and beneficial occupancy of the 110,000 square foot office addition, FDOT shall review actual conditions on-site and take appropriate corrective action including, but

not limited to, acquiring additional land for remote parking facilities and promoting increased participation in van/car pool programs.

(k) Sidewalks, Trails and Bikeways: FDOT shall construct sidewalks along Winnemissett, Alabama and Volusia Avenues according to City or FDOT standards, at the City's option. FDOT shall make adequate provision for bicycle parking on-site to promote greater use of alternative transportation modes.

(l) Screening: FDOT shall enter into a perpetual maintenance agreement with the City to maintain any landscape areas required or permitted to encroach upon any City right-of-way.

(m) Binding Effect of Plans; Recording: The provisions of the foregoing Development Plan, as amended, including any and all supplementary orders and resolutions, shall bind and inure to the benefit of FDOT and its officers, agents, employees, sublessees, successors and assigns. A copy of this Ordinance and all subsequent orders and resolutions shall be filed for record in the Official Records of Volusia County, Florida.

(n) Reverter Provision: The City Commission may rezone any portion of the project which has not secured final site plan approval on or before December 31, 1992 as may be necessary or appropriate to protect adjoining properties or the public health, safety and welfare, unless the City Commission, for good cause shown, shall extend the time period indicated in this paragraph.

Section 6. The City Engineer, Planning Director and Building Department are hereby directed to change the Comprehensive Zoning Map of the City of DeLand to conform with the change of zoning enacted in this Ordinance.

Section 7. No rights to obtain final development orders nor any rights to develop the property have been granted or implied by this Ordinance. Final development orders for the property including, but not limited to, site plan approval shall be subject to a determination of capacity.

Section 8. If any section, sentence, clause or phrase of this Ordinance is held to be invalid or unconstitutional by any court of competent jurisdiction, that holding shall in no way affect the validity of the remaining portions of this Ordinance.

Section 9. This Ordinance shall become effective immediately upon its adoption.

PASSED AND DULY ADOPTED this 21st day of October, 1991.



Wiley M. Nash
Mayor-Commissioner

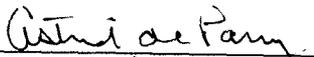
ATTEST:



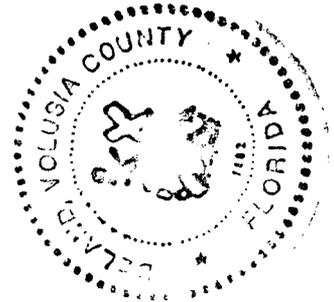
Viola H. Ballentine
City Clerk - Auditor

Passed on first reading: October 7, 1991
Adopted on second reading: October 21, 1991

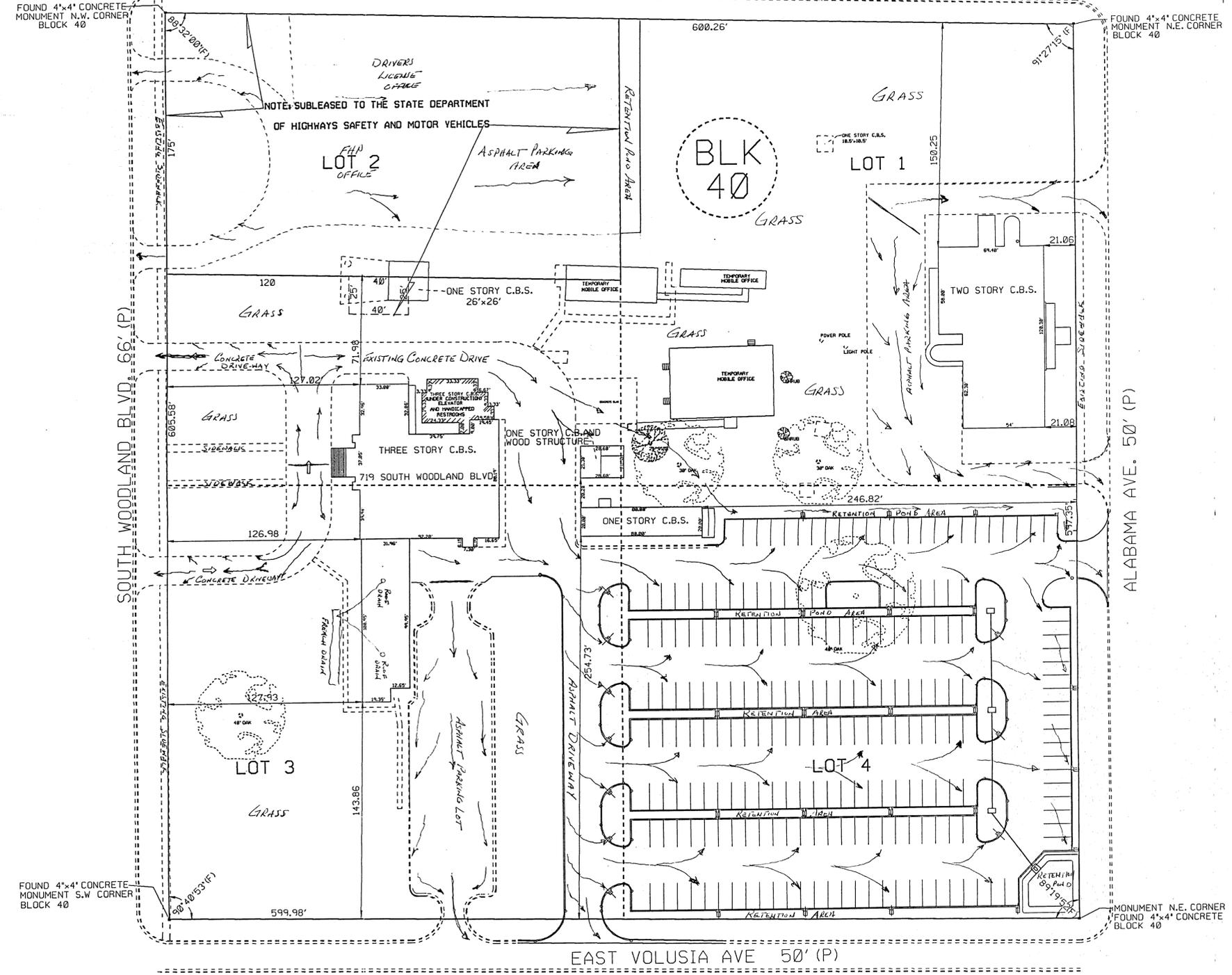
APPROVED AS TO FORM AND LEGALITY:



Astrid de Parry
City Attorney



EAST WINNEMISSETT AVE. 50' (P)



LEGAL DESCRIPTION

ACCORDING TO DEED BOOK 1073, PAGE 244 AND OFFICIAL RECORDS BOOK 1708, PAGE 0998 ALL OF BLOCK 40, HOWRYS ADDITION TO DELAND ACCORDING TO A MAP THEREOF RECORDED IN MAP BOOK 1, PAGE 37, VOLUSIA COUNTY, FLORIDA CONTAINING 8.29 ACRES MORE OR LESS

SUBJECT TO SUBLEASE TO THE STATE DEPARTMENT OF HIGHWAY SAFETY AND MOTOR VEHICLES, DATED AUGUST 17, 1983, DESCRIBED AS FOLLOWS: THE NORTH 175 FEET OF LOT 2, BLOCK 40 HOWRYS ADDITION TO DELAND ACCORDING TO A MAP THEREOF RECORDED IN MAP BOOK 1, PAGE 37

AND; SUBJECT TO A SUBLEASE TO THE STATE DEPARTMENT OF HIGHWAY SAFETY AND MOTOR VEHICLES, DATED OCTOBER 16, 1974 DESCRIBED AS FOLLOWS:

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I HEREBY CERTIFY THAT TO THE BEST OF MY KNOWLEDGE AND BELIEF THIS DRAWING, CONSISTING OF SHEET 1, IS A TRUE, ACCURATE AND COMPLETE DEPICTION OF A FIELD SURVEY PERFORMED UNDER MY DIRECTION AND COMPLETED AUGUST, 1991.

I FURTHER CERTIFY THAT SAID DRAWING IS IN COMPLIANCE WITH THE MINIMUM TECHNICAL STANDARDS FOR BOUNDARY SURVEYS AS SET FORTH BY THE FLORIDA BOARD OF LAND SURVEYORS.

Carl O'Steen
CARL O'STEEN
FLORIDA REGISTERED LAND SURVEYOR NO. 4072
719 SOUTH WOODLAND BLVD.
DELAND, FLORIDA 32720
DATE: 8/12/91

NOT VALID UNLESS SIGNED AND SEALED

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED
Date: 10/21/91
Approval Expires: 12/31/92
H.M. Schmitt
REFER TO SP-2-91-52-19

RECEIVED
SEP 6 1991

Exhibit 3

FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY SURVEYING AND MAPPING

BOUNDARY SURVEY

STATE ROAD NO. N/A VOLUSIA COUNTY

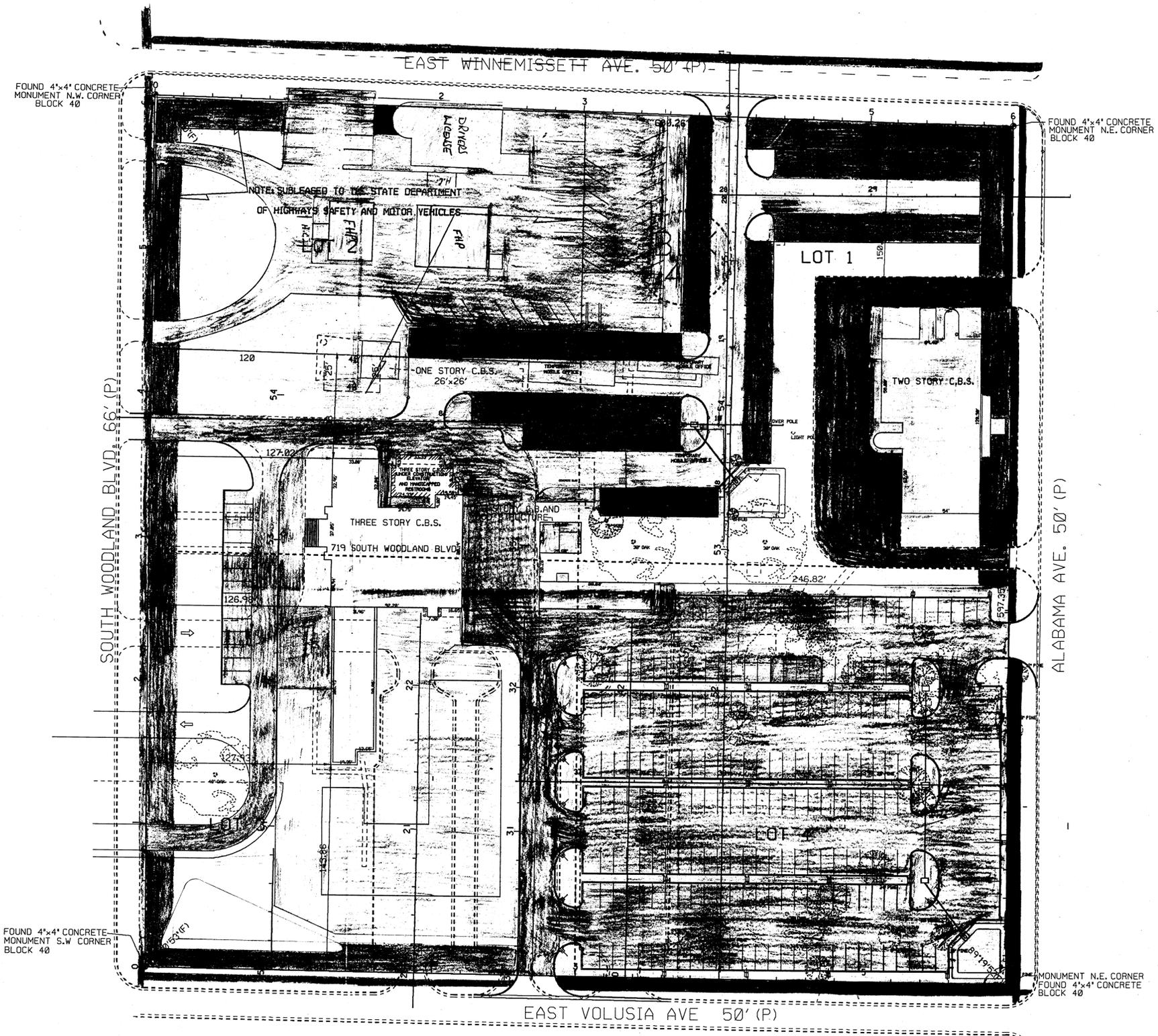
DRAWN	CHM	8/91
TRACED	CHM	8/91
CHECKED	CHM	8/91
MAPS PREPARED BY	CADD DEPT.	
FIELD BOOK NO.	899056	
W.P.I. NO. N/A	SCALE:	1"=40'
SECTION	N/A	SHEET 1 OF 1

REVISION	BY	DATE

EXISTING DRAINAGE PLAN

SECTION-16, TOWNSHIP-17-SOUTH, RANGE-30-EAST

Ordinance 91-54
Exhibit A



LEGAL DESCRIPTION

ACCORDING TO DEED BOOK 1073, PAGE 244 AND OFFICIAL RECORDS BOOK 1708, PAGE 0998 ALL OF BLOCK 40, HOWRY'S ADDITION TO DELAND ACCORDING TO A MAP THEREOF RECORDED IN MAP BOOK 1, PAGE 37, VOLUSIA COUNTY, FLORIDA CONTAINING 8.29 ACRES MORE OR LESS

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SCALE: 1"=40'

I HEREBY CERTIFY THAT TO THE BEST OF MY KNOWLEDGE AND BELIEF THIS DRAWING, CONSISTING OF SHEET 1, IS A TRUE, ACCURATE AND COMPLETE DEPICTION OF A FIELD SURVEY PERFORMED UNDER MY DIRECTION AND COMPLETED AUGUST, 1991.

I FURTHER CERTIFY THAT SAID DRAWING IS IN COMPLIANCE WITH THE MINIMUM TECHNICAL STANDARDS FOR BOUNDARY SURVEYS AS SET FORTH BY THE FLORIDA BOARD OF LAND SURVEYORS,

RECEIVED
OCT 7 1991

Exhibit 8

CARL O'STEEN
FLORIDA REGISTERED LAND SURVEYOR NO. 4072
719 SOUTH WOODLAND BLVD.
DELAND, FLORIDA 32720
DATE:

NOT VALID UNLESS SIGNED AND SEALED

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED
Date: 10/21/91
Approval Engineer: [Signature]
11-M. Deland
REFER TO SP. 2-91-08-19

FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY SURVEYING AND MAPPING			
BOUNDARY SURVEY			
STATE ROAD NO. N/A		VOLUSIA COUNTY	
BY	DATE		
DRAWN	CHM 8/91		
TRACED	CHM 8/91		
CHECKED	CHM 8/91		
MAPS PREPARED BY		FIELD BOOK NO. 'S	
CADD DEPT.		899856	
W.P.I. NO. N/A		SCALE: 1"=40'	
SECTION	N/A	SHEET 1 OF 1	

SECTION-16, TOWNSHIP-17-SOUTH, RANGE-30-EAST

FOUND 4"x4" CONCRETE MONUMENT N.W. CORNER BLOCK 40

FOUND 4"x4" CONCRETE MONUMENT N.E. CORNER BLOCK 40

FOUND 4"x4" CONCRETE MONUMENT S.W. CORNER BLOCK 40

MONUMENT N.E. CORNER FOUND 4"x4" CONCRETE BLOCK 40

SITE PLAN - EXISTING PARKING & PROPOSED PARKING

EAST WINNEMISSETT AVE. 50' (P)

FOUND 4"x4" CONCRETE MONUMENT N.W. CORNER BLOCK 40

FOUND 4"x4" CONCRETE MONUMENT N.E. CORNER BLOCK 40

NOTE: SUBLEASED TO THE STATE DEPARTMENT OF HIGHWAYS SAFETY AND MOTOR VEHICLES

LOT 2

BLK 40

LOT 1

LEGAL DESCRIPTION

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SOUTH WOODLAND BLVD. 66' (P)

ALABAMA AVE. 50' (P)

LOT 3

LOT 4

SCALE: 1"=40'

I HEREBY CERTIFY THAT TO THE BEST OF MY KNOWLEDGE AND BELIEF THIS DRAWING, CONSISTING OF SHEET 1, IS A TRUE, ACCURATE AND COMPLETE DEPICTION OF A FIELD SURVEY PERFORMED UNDER MY DIRECTION AND COMPLETED AUGUST, 1991.

I FURTHER CERTIFY THAT SAID DRAWING IS IN COMPLIANCE WITH THE MINIMUM TECHNICAL STANDARDS FOR BOUNDARY SURVEYS AS SET FORTH BY THE FLORIDA BOARD OF LAND SURVEYORS.

Carl O'Steen
CARL O'STEEN
FLORIDA REGISTERED LAND SURVEYOR NO. 4072
719 SOUTH WOODLAND BLVD.
DELAND, FLORIDA 32720
DATE: 8/12/91

NOT VALID UNLESS SIGNED AND SEALED

RECEIVED
SEP 25 1991

Exhibit 7A

FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY SURVEYING AND MAPPING

BOUNDARY SURVEY

STATE ROAD NO. N/A VOLUSIA COUNTY

BY	DATE
DRAWN CHM	8/91
TRACED CHM	8/91
CHECKED CHM	8/91

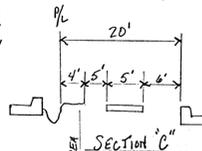
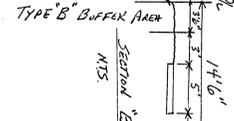
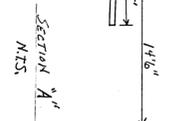
MAPS PREPARED BY CADD DEPT. FIELD BOOK NO. 899056

W.P.I. NO. N/A SCALE: 1"=40'

SECTION N/A SHEET 1 OF 1

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED
Date: 10/21/91
Approval Expires: 12/31/92
J. M. Schindler
REFER TO SHEET 2-97-08-19

REVISION	BY	DATE



EAST VOLUSIA AVE 50' (P)

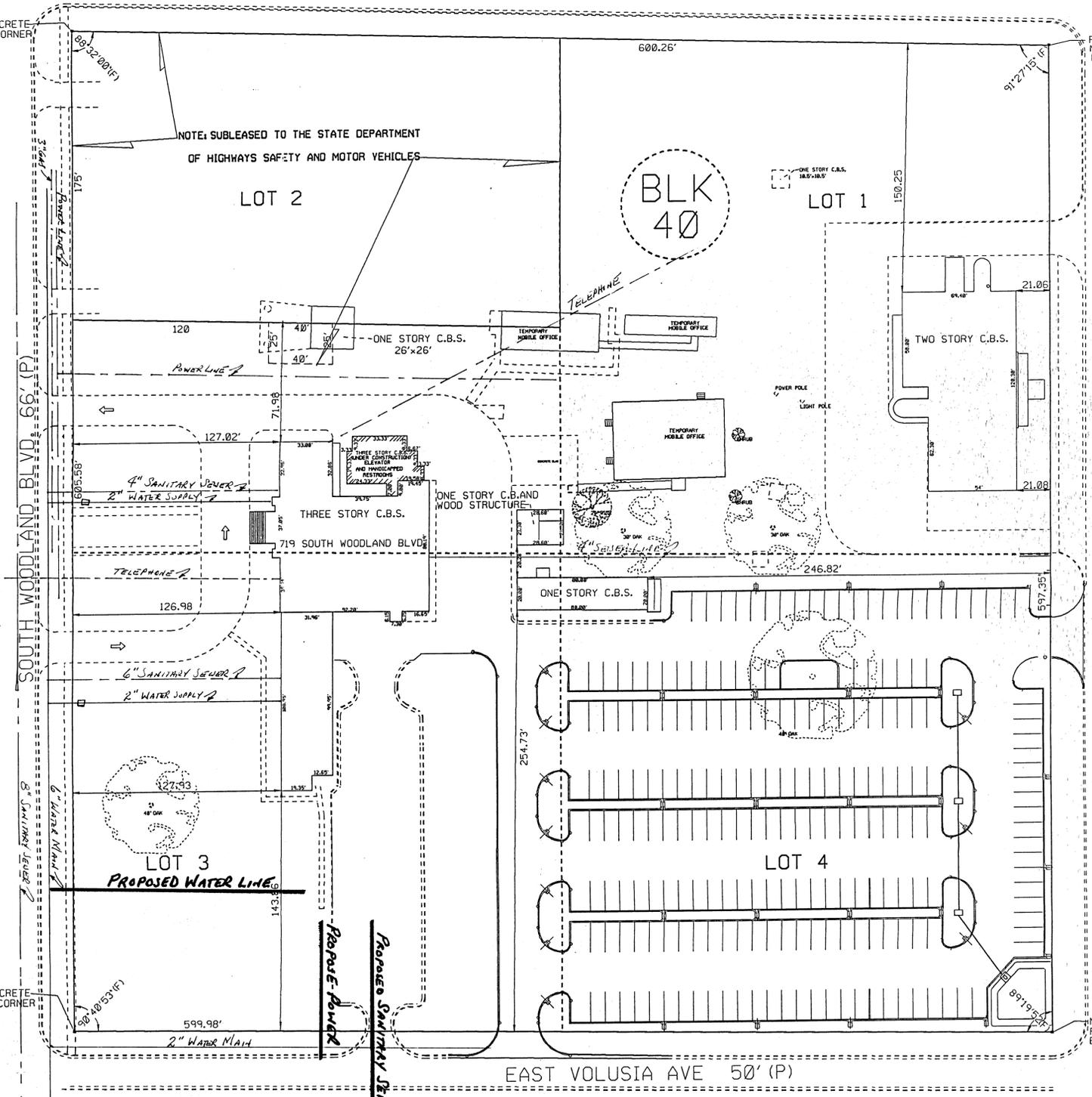
PROPOSED SIDEWALK PLAN

SECTION-16, TOWNSHIP-17-SOUTH, RANGE-30-EAST

EAST WINNEMISSETT AVE. 50' (P)

FOUND 4"x4" CONCRETE MONUMENT N.W. CORNER BLOCK 40

FOUND 4"x4" CONCRETE MONUMENT N.E. CORNER BLOCK 40



LEGAL DESCRIPTION

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Carl O'Steen
 CARL O'STEEN
 FLORIDA REGISTERED LAND SURVEYOR NO. 4072
 719 SOUTH WOODLAND BLVD.
 DELAND, FLORIDA 32720
 DATE: 8/12/91

NOT VALID UNLESS SIGNED AND SEALED

CITY OF DELAND
 PLANNING DEPARTMENT
APPROVED

Date: 10/21/91
 Approval Expires: 12/31/92
1911 Deland
 REFER TO SP-2-91-08-19

RECEIVED
 SEP 10 1991

Exhibit 7

FLORIDA DEPARTMENT OF TRANSPORTATION
 RIGHT OF WAY SURVEYING AND MAPPING

BOUNDARY SURVEY

STATE ROAD NO. N/A VOLUSIA COUNTY

BY	DATE
CHM	8/91
CHM	8/91
CHM	8/91

MAPS PREPARED BY: CADD DEPT. FIELD BOOK NO. 899856

W.P.I. NO. N/A SCALE: 1"=40'

SECTION N/A SHEET 1 OF 1

EXISTING UTILITY LOCATION PLAN

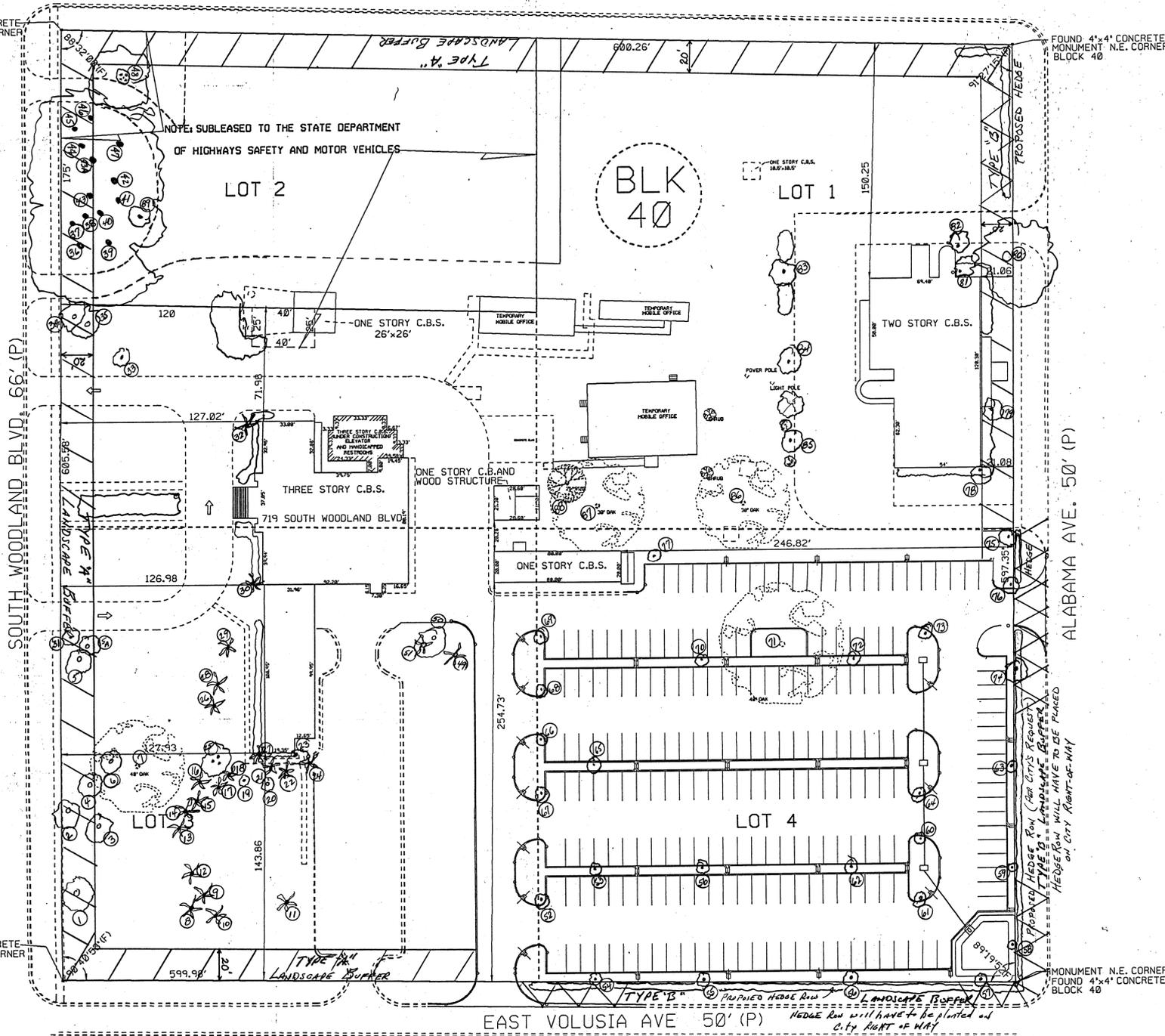
SECTION-16, TOWNSHIP-17-SOUTH, RANGE-30-EAST

REVISION	BY	DATE

EAST WINNEMISSETT AVE. 50' (P)

FOUND 4"x4" CONCRETE MONUMENT N.W. CORNER BLOCK 40

FOUND 4"x4" CONCRETE MONUMENT N.E. CORNER BLOCK 40



LEGAL DESCRIPTION

ACCORDING TO DEED BOOK 1073, PAGE 244 AND OFFICIAL RECORDS BOOK 1708, PAGE 0998 ALL OF BLOCK 40, HOWRYS ADDITION TO DELAND ACCORDING TO A MAP THEREOF RECORDED IN MAP BOOK 1, PAGE 37, VOLUSIA COUNTY, FLORIDA CONTAINING 8.29 ACRES MORE OR LESS

SUBJECT TO SUBLEASE TO THE STATE DEPARTMENT OF HIGHWAY SAFETY AND MOTOR VEHICLES, DATED AUGUST 17, 1983, DESCRIBED AS FOLLOWS: THE NORTH 175 FEET OF LOT 2, BLOCK 40 HOWRYS ADDITION TO DELAND ACCORDING TO A MAP THEREOF RECORDED IN MAP BOOK 1, PAGE 37

AND: SUBJECT TO A SUBLEASE TO THE STATE DEPARTMENT OF HIGHWAY SAFETY AND MOTOR VEHICLES, DATED OCTOBER 16, 1974 DESCRIBED AS FOLLOWS:

LOT 2, BLOCK 40, HOWRYS ADDITION TO DELAND FLORIDA, FLORIDA ACCORDING TO PLAT RECORDED IN MAP BOOK 1, PAGE 37, PUBLIC RECORDS OF VOLUSIA COUNTY FLORIDA, DESCRIBED AS FOLLOWS: COMMENCE AT THE NORTHWEST CORNER OF SAID LOT 2, THENCE RUN SOUTH ALONG THE WEST LINE OF SAID LOT 2, A DISTANCE OF 175 FEET, THENCE RUN EAST, PARALLEL TO THE NORTH LINE OF SAID LOT 2 A DISTANCE OF 120' FROM THE POINT OF BEGINNING, THENCE RUN SOUTH 25 FEET, EAST 40 FEET, NORTH 25 FEET AND WEST 40 FEET, TO THE POINT OF BEGINNING

SCALE: 1"=40'

I HEREBY CERTIFY THAT TO THE BEST OF MY KNOWLEDGE AND BELIEF THIS DRAWING, CONSISTING OF SHEET 1, IS A TRUE, ACCURATE AND COMPLETE DEPICTION OF A FIELD SURVEY PERFORMED UNDER MY DIRECTION AND COMPLETED AUGUST, 1991.

I FURTHER CERTIFY THAT SAID DRAWING IS IN COMPLIANCE WITH THE MINIMUM TECHNICAL STANDARDS FOR BOUNDARY SURVEYS AS SET FORTH BY THE FLORIDA BOARD OF LAND SURVEYORS.

Carl O'Steen
CARL O'STEEN
FLORIDA REGISTERED LAND SURVEYOR NO. 4072
719 SOUTH WOODLAND BLVD.
DELAND, FLORIDA 32720
DATE: 8/12/91

NOT VALID UNLESS SIGNED AND SEALED

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED
Date: 10/21/91
Approved: *[Signature]*
REFER TO SP-2-91-05-19

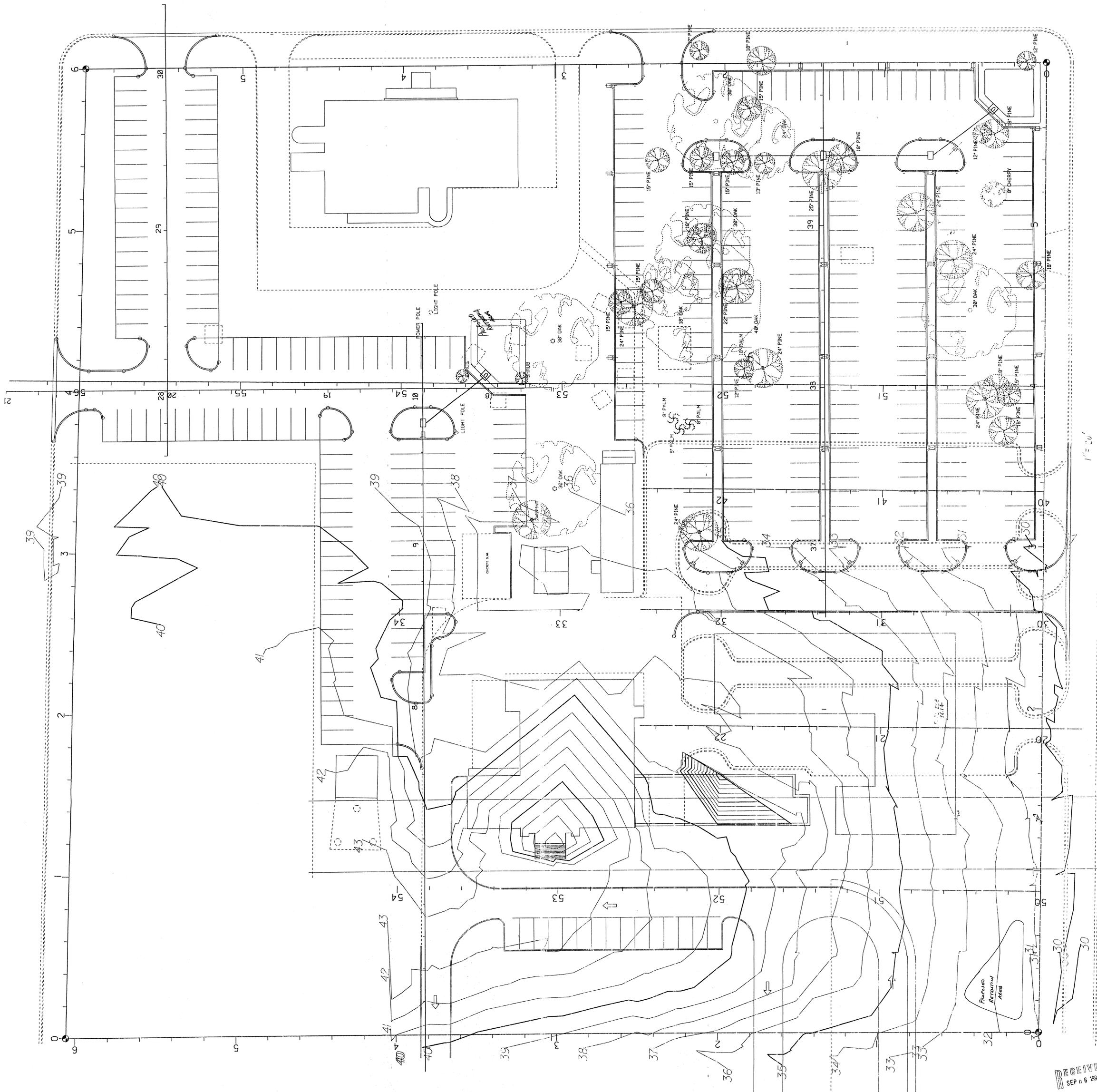
Exhibit 6

FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY SURVEYING AND MAPPING			
BOUNDARY SURVEY			
STATE ROAD NO. N/A	VOLUSIA COUNTY		
BY	DATE		
DRAWN CHM	8/91		
TRACED CHM	8/91		
CHECKED CHM	8/91		
MAPS PREPARED BY		FIELD BOOK NO. 899856	
CADD DEPT.			
W.P.I. NO. N/A	SCALE: 1"=40'		
SECTION N/A	SHEET 1 OF 1		

PROPOSED LANDSCAPE PLAN
TYPE "A" PART OF VOLUNIA, WOODLAND & WINNEMISSETT AVE
TYPE "B" PART OF VOLUNIA & ALABAMA AVE
SECTION-16, TOWNSHIP-17-SOUTH, RANGE-30-EAST

PROPOSED LANDSCAPE PLAN

RECEIVED
SEP 25 1991



TOPOGRAPHY MAP
AND
PROPOSED DRAINAGE PLAN

Exhibit 4

RECEIVED
SEP 6 1991

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED
Date: 10/21/91
Approval Expires: 2/31/92
17M. Schmitt
REFER TO SP-2-91-08-19

04-Sep-91

PROPOSED TREE INVENTORY
FDOT PROPERTY - DeLAND

(TREESOR1.WK1)

LOCATION NUMBER	TYPE	SIZE DBH	SPECIMEN	NON-SPECIMEN
25	CAMPHOR	5		X
82	CAMPHOR	3		X
89	CEDAR	6		X
20	CREPE MYRTLE	2		X
28	CREPE MYRTLE	3		X
29	CREPE MYRTLE	3		X
19	CREPE MYRTLE	3		X
31	HOLLY	3		X
33	MAGNOLIA	2		X
36	OAK	26	X	
37	OAK	17		X
38	OAK	20	X	
39	OAK	32	X	
40	OAK	13		X
41	OAK	15		X
42	OAK	12		X
43	OAK	15		X
43A	OAK	16		X
44	OAK	11		X
45	OAK	15		X
46	OAK	19	X	
47	OAK	30	X	
52	OAK	2		X
53	OAK	2		X
54	OAK	2		X
55	OAK	2		X
56	OAK	2		X
56A	OAK	2		X
57	OAK	2		X
58	OAK	DEAD		X
59	OAK	1.5		X
60	OAK	2		X
61	OAK	DEAD		X
62	OAK	2		X
63	OAK	1.5		X
64	OAK	2		X
65	OAK	2		X
66	OAK	2		X
67	OAK	2		X
68	OAK	2		X
69	OAK	3		X
7	OAK	40	X	
70	OAK	1.5		X
71	OAK	40	X	
72	OAK	1.5		X
73	OAK	DEAD		X
75	OAK	1.5		X
76	OAK	2		X
80	OAK	35	X	
83	OAK	6		X
86	OAK	30	X	
87	OAK	30	X	
48	OAK CLUMP	21	X	

(TO BE RELOCATED/REMOVED)

(TO BE RELOCATED/REMOVED)

48	OAK CLUMP	21	X		
48	OAK CLUMP	21	X		
48	OAK CLUMP	12		X	
48	OAK CLUMP	12		X	
77	ORANGE	3		X	
10	PALM	11		X	(TO BE RELOCATED/REMOVED)
11	PALM	13		X	(TO BE RELOCATED/REMOVED)
12	PALM	11		X	(TO BE RELOCATED/REMOVED)
13	PALM	12		X	(TO BE RELOCATED/REMOVED)
14	PALM	9		X	(TO BE RELOCATED/REMOVED)
15	PALM	10		X	(TO BE RELOCATED/REMOVED)
16	PALM	13		X	(TO BE RELOCATED/REMOVED)
17	PALM	13		X	(TO BE RELOCATED/REMOVED)
18	PALM	12		X	(TO BE RELOCATED/REMOVED)
21	PALM	10		X	(TO BE RELOCATED/REMOVED)
22	PALM	10		X	(TO BE RELOCATED/REMOVED)
24	PALM	10		X	(TO BE RELOCATED/REMOVED)
27	PALM	10		X	(TO BE RELOCATED/REMOVED)
30	PALM	10		X	
32	PALM	9		X	
49	PALM	10		X	
51	PALM	10		X	
8	PALM	11		X	(TO BE RELOCATED/REMOVED)
9	PALM	13		X	(TO BE RELOCATED/REMOVED)
1	PINE	16		X	(TO BE REMOVED)
2	PINE	15		X	(TO BE REMOVED)
26	PINE	13		X	
3	PINE	14		X	(TO BE REMOVED)
34	PINE	15		X	
35	PINE	15		X	
4	PINE	15		X	(TO BE REMOVED)
5	PINE	17		X	
5A	PINE	15		X	
6	PINE	17		X	
74	PINE	19		X	
88	PINE	24		X	
50	SWEET GUM	12		X	
23	LAUREL CHERRY	6.5		X	(TO BE REMOVED)
78	LAUREL CHERRY	8		X	
78	LAUREL CHERRY	8		X	
79	LAUREL CHERRY	10		X	
81	LAUREL CHERRY	4		X	
84	LAUREL CHERRY	4		X	
85	LAUREL CHERRY	6		X	
85	LAUREL CHERRY	6		X	
	CAMPHOR	8			
	CEDAR	6			
	CREPE MYRTLE	11			
	HOLLY	3			
	MAGNOLIA	2			
	OAK	549.5			
	ORANGE	3			
	PALM	207			
	PINE	195			
	SWEET GUM	12			
	LAUREL CHERRY	52.5			
	TOTAL	1049			

TOTAL DBH = 1049
 DBH REM./RELOC. 244
 DBH REMAINING = 805

The News-Journal

Published Daily and Sunday
Daytona Beach, Volusia County, Florida

State of Florida,
County of Volusia:

Before the undersigned authority personally appeared

C. Morgan Miller

who, on oath says that he is.....

Classified Manager

of The News-Journal, a daily and Sunday newspaper, published
at Daytona Beach in Volusia County, Florida; that the
attached copy of advertisement, being a.....

Notice of Public Hearing

in the matter of.....

Ordinance Number 91-54

in the Court, was published

in said newspaper in the issues.....

October 11, 1991

Affiant further says that The News-Journal is a newspaper
published at Daytona Beach, in said Volusia County, Florida,
and that the said newspaper has heretofore been continuously
published in said Volusia County, Florida, each day and
Sunday and has been entered as second-class mail matter at the
post office in Daytona Beach, in said Volusia County, Florida,
for a period of one year next preceding the first publication of
the attached copy of advertisement; and affiant further says
that he has neither paid nor promised any person, firm or
corporation any discount, rebate, commission or refund for the
purpose of securing this advertisement for publication in the
said newspaper.

C. Morgan Miller

Sworn to and subscribed before me

this 11th day of October.....

A.D. 19 91



NOTARY PUBLIC, State of Florida at Large
My Commission Expires February 23, 1993

LEGAL ADVERTISEMENT

NOTICE OF PUBLIC HEARING

CITY OF DELAND, FLORIDA
HEREBY GIVEN that
there will be a Public Hearing on the
following ordinance on October 21,
1991, at the regular meeting of the
DeLand City Commission. The meet-
ing will take place at 7:00 P.M. in the
City Commission Chambers, 120
South Florida Avenue. All interested
parties are invited to attend and be
heard on the matter.
Any person who appeals any decision
made by the City Commission with
respect to any matter considered at
this meeting will need a record of the
proceeding and, thus, may need to
ensure that a verbatim record of the
proceeding is made, including the tes-
timony and evidence upon which an

appeal may be based.
The original of the ordinance is on file
in the office of the City Clerk and may
be inspected Monday through Friday
from 8:00 A.M. to 4:30 P.M.

ORDINANCE 91-54

AN ORDINANCE OF THE CITY
OF DELAND, FLORIDA, CHANG-
ING THE ZONING FROM R1-B,
SINGLE FAMILY RESIDENTIAL
DISTRICT, TO PD, PLANNED
DEVELOPMENT DISTRICT, ON
DESCRIBED PROPERTY
LOCATED ON THE EAST SIDE
OF SOUTH WOODLAND BOULE-
VARD BOUNDED BY EAST WIN-
NEMISSETT AVENUE TO THE
NORTH, ALABAMA AVENUE TO
THE EAST AND VOLUSIA AVE-
NUE TO THE SOUTH, APPROV-
ING A DEVELOPMENT PLAN
FOR THE FLORIDA DEPART-
MENT OF TRANSPORTATION
DISTRICT OFFICE COMPLEX, A
PLANNED DEVELOPMENT CON-
SISTING OF APPROXIMATELY
8.29 ACRES OF PROPERTY
LOCATED ON THE EAST SIDE
OF SOUTH WOODLAND BOULE-
VARD BETWEEN EAST WINNE-
MISSETT AVENUE, ALABAMA
AVENUE AND EAST VOLUSIA
AVENUE; DIRECTING A
CHANGE IN THE COMPREHEN-
SIVE ZONING MAP; CONDITION-
ING FINAL DEVELOPMENT
APPROVAL ON A DETERMIN-
ATION OF CAPACITY; PROVID-
ING FOR SEVERABILITY AND
AN EFFECTIVE DATE.

Legal L02677, October 11, 1991 1-t.

FDOT Planned Development
719 S. Woodland Boulevard

List of Exhibits

- Exhibit 1 - DOT Development Plan Report (09/24/91)
- Exhibit 2 - DOT Letter to Turgut Dervish
Re: Traffic impact study requirements
- Exhibit 3 - Drainage plan (9/16/91)
- Exhibit 4 - Topography Map and Proposed Drainage Plan (9/16/91)
- Exhibit 5 - Tree Survey (9/16/91)
- Exhibit 6 - Proposed Landscape Plan (9/25/91)
- Exhibit 7 - Existing Utility Plan (9/10/91)
- Exhibit 7A- Proposed Sidewalk Plan (9/25/91)
- Exhibit 8 - Overall (color coded) Site Plan (8/27/91)
- Exhibit 9 - Memo to Gary form Turgut Dervish (9/9/91)
Re: Trip Generation Rate

CITY OF DELAND
PLANNING DEPARTMENT

APPROVED

Date: 10/21/91

Approval Expires: 12/31/92

M. M. Scendall
MEMO TO SP-2-91-08-19

ZONE\EXHIBITS

Exhibit 1

Department of Transportation Development Plan Report (09/24/91)

The existing facilities on our property located at 719 South Woodland Boulevard are not adequate to house District Administrative staff totalling approximately 374 people at present. The primary purpose of the planned development district is to provide an additional 110,000 sq.ft. of office space & parking and upgrade the existing office building on the site. This will allow the Department to consolidate functions & personnel presently housed in commercial leased space in 4 separate locations within DeLand city limits. It will also allow the Department to remove the temporary office trailers on site and bring the existing building up to current code requirements. It is expected that this development effort will improve the productivity of the Department by improving coordination among the organizational units presently located at remote sites and eliminate much of the in city travel of Department personnel.

The Department has \$7.8 million in current year funds to design and construct 110,000 additional square footage and increase the amount of on-site parking spaces. An additional \$3.0 million is in the fixed capital outlay budget request for 92/93 to renovate the existing facility. The Department has retained the services of Sverdrup Corporation to develop a Request for Proposal package to obtain a Design/Build team. The design/build concept is a method whereby competitive bids are received to design and build a facility that meets specific design criteria for a fixed price. This method was chosen rather than the traditional method of contracting with a architect for design services and general contractor for construction because it has the following advantages:

- * less expensive way to build a facility
- * improved accountability by having a relationship with one legal entity rather than two
- * shorter time frame for project because some steps can be overlapped more effectively

The schedule for the proposed project and the requested statistical information are attached. Specific plans and standards will not be available until a later date because they are the product of the design criteria set forth in the request for proposal or the selection of the design/build team. The development plan map illustrates the broad conceptual design and indicates key characteristics. Briefly the key points of this development plan are:

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED

Date: 10/21/91

Approval Expires: 12/31/92

N.M. Sclendh
1001 33-2-91-08-19

1. SETBACK REQUIREMENTS:

FDOT will adhere to the following setback requirements:

- a. Minimum of twenty-five (25) feet from East Volusia Avenue.
- b. Minimum of seventy-five (75) feet from South Woodland Boulevard.

2. LANDSCAPE BUFFER:

Type "A" Landscape Area:

FDOT will adhere to the following landscape buffer requirements:

- a. A twenty (20) foot wide buffer zone will be established for the following locations on FDOT property:
 1. Frontage along South Woodland Boulevard from East Volusia Avenue to East Winnemissett Avenue.
 2. Frontage along South Volusia Avenue from South Woodland Boulevard to FDOT existing parking area. (See Item No. 10 External Compatibility Plan.)
 3. Frontage along East Winnemissett Avenue from South Woodland Boulevard to Alabama Avenue. (Except area along side of Driver's License building.)
 4. Frontage along Alabama Avenue from East Winnemissett Avenue to FDOT existing parking area. (See Item No. 10 External Compatibility Plan.)

The area of Alabama Avenue and East Volusia Avenue bordering FDOT's existing parking area has been planted with oak trees along the berm of the existing retention area. These oak trees meet the current standards set by the City for DBH and height requirements. Spacing of the oak trees around the perimeter of FDOT existing parking area is between 30 - 50 feet. This spacing should be adequate since FDOT will be planting a new visual barrier hedge in this area.

FDOT has eleven (11) specimen trees on the property and we plan to preserve all of them. The calculated DBH is 867 for the property which we will meet. The spacing of trees in the buffer zone will be a minimum of 30 feet. It is our intent to transplant existing non-specimen trees when practical, if they are in the construction zone. (See Tree Protection Calculation forms.)

The landscaping in front of the building will consist of trees, hedges, shrubs, grass, ground cover, planting beds, and walkways and be more formal than other landscaping on the property. It will be a city park like setting and may have some focal point (i.e. fountain, large sculpture, etc.). It may have some outdoor seating and outdoor lighting. The intent will be to create a professional pleasant front entryway to the site.

Type "B" Landscape Area:

At the request of the City Planning Department, FDOT will plant and maintain at its own expense a visual barrier for screening the Department's existing parking area from Earl Brown Park and residential houses located on East Volusia Avenue. Due to the location of the existing parking area and stormwater retention areas, this hedge will have to be planted on city property. The City will have to approve FDOT's proposal to use the right-of-way for landscaping buffering. Before FDOT plant this visual barrier on city property.

The proposed visual barrier will be planted from the intersection of Alabama Avenue and East Volusia Avenue north along Alabama Avenue to the existing drive exiting the District Soil Lab, then from the existing drive entering the District Soil Lab to the intersection of Alabama Avenue and East Winnemissett Avenue, then from the intersection of Alabama Avenue and East Volusia Avenue west along East Volusia to the existing drive. See Item No. 10 "External Compatibility Plan" for specifics concerning this planting.

FDOT has been maintaining the City right-of-way that abuts our property along East Volusia Avenue, Alabama Avenue and East Winnemissett Avenue for numerous years. FDOT will continue to maintain these right-of-ways after the proposed visual barrier has been planted as requested by the City.

FDOT will enter into a perpetual maintenance agreement with the City indicating that FDOT will be responsible for maintaining landscape areas required on City right-of-way for Volusia Avenue and Alabama Avenue. FDOT attorneys will prepare this agreement for submittal with final site plan review.

3. PROPOSED HEIGHT REQUESTS:

FDOT preliminary plans call for a six (6) story structure to be constructed on their property located at 719 South Woodland Boulevard. Due to the height required to build this six (6) story structure. FDOT is proposing a height of 85 feet, excluding elevator penthouse and parapet walls, etc.

4. ENVIRONMENTAL IMPACT:

The site of the new addition to the Department's District Office is presently in parking and landscaped grounds. It is considered an urban environment, zoned for commercial use. As such, the new addition will not affect the overall character of the Flora and Fauna communities.

5. TREE PROTECTION:

At time of site plan review, FDOT will comply with Ordinance 89-24 concerning applicable tree protection.

FDOT will not be removing any specimen trees from this site. Currently FDOT has 11 trees on site that are specimen trees and 78 other type trees.

FDOT will protect all trees except the ones that will fall within the footprint of the new building, new parking, or drainage retention. Any trees required to be removed may be relocated on site, if Design/Build contractor proposes to do so in his landscape plan. Even if such trees are not relocated on site at the time of site plan review, the plans will either meet all minimum tree protection requirements or FDOT will seek all applicable variances.

Refer to attached work sheets for calculations used in determining tree requirements for FDOT property.

6. DRAINAGE PLAN:

Currently FDOT is retaining 80 percent of the stormwater runoff generated on their property. With the construction of the new building and parking areas, new retention areas will be constructed to handle the additional anticipated runoff. It is estimated that FDOT will retain 95 percent of the stormwater runoff, after the construction of the new retention areas. The construction of new retention area will create a decrease of 15 percent in the amount of stormwater that currently drains towards the city streets and US 17/92.

FDOT is proposing to utilize a pervious paving system for much of its proposed parking area and walkways. This will reduce the amount of retention required for runoff.

At time of final site plan submittal FDOT will submit to the City a copy of the final drainage design plan and calculations as developed by the Design/Build contractor for the project.

Final stormwater retention plan will meet all applicable City requirements. FDOT obtained approval from the St. Johns Water Management District for the proposed drainage plan at the same time as they obtained approval for the parking area constructed approximately 2-3 years ago on FDOT property.

7. SECURITY FENCING:

Any fencing proposed by FDOT along the perimeter of the property will be placed behind a hedge or other suitable screening material to make it obscure to the public or residents in the area. Fencing will be six (6) foot chain-link with no barbed wire around the top. No fencing will be placed along Woodland Boulevard in front of existing buildings.

8. SECURITY LIGHTING:

FDOT may be providing security lighting for its buildings and parking areas, for the safety of our employees who may have to work after sunset.

9. PARKING STANDARDS:

Current City of DeLand regulation require one (1) parking space for every 250 gross sq.ft. of building. FDOT will have approximately 142,000 sq.ft. on site upon the completion of the new addition. Based on City requirements, that means we will need 568 parking spaces on site.

Upon completion of the new addition FDOT will have approximately 499 employees reporting to this site. The total positions of 499 includes 40 positions that will be assigned to this complex over the next five (5) years.

FDOT has performed their own parking analysis (see Exhibit "A") and have concluded that we will need 473 total spaces for employee, visitor and pool car parking once the site becomes fully occupied. The number of required parking spaces was calculated based on the existing parking requirements. Currently the number of employee vehicles equal 80 percent of the number of total positions or 400 parking spaces, plus 30 pool cars and 18 for visitor parking for a grand total of 448 spaces. The 80 percent is used because of vacancies, annual and sick leave, out of town travel and car pooling. The 80 percent comes from current Institute of Traffic Engineers Guidelines.

FDOT will use 85 percent of the total positions for design purpose in determining the total parking spaces required for employee parking. Eighty-five (85) percent of our total positions is 425 parking spaces for employee parking, plus 30 spaces for pool cars and 18 for visitor parking for a total parking spaces required of 473.

FDOT currently has 248 parking spaces on site. When the new parking area is completed an additional 152 parking spaces will become available for a total parking capability of 400 on site. These 400 parking spaces should be enough to adequately handle all of FDOT parking requirements until additional land is acquired.

FDOT will utilize current department standards for the design of parking criteria (space width, aisle width, handicapped spaces). FDOT parking plan will provide plan and profile sheets for the proposed parking area. This plan will designate what areas will be pervious and impervious.

Acquisition of additional land adjacent to this property will be pursued over time to provide additional parking as needed.

10. EXTERNAL COMPATIBILITY PLAN:

The Planned Development regulations state that the PD report must address external compatibility. This refers to how the proposed development relates to the adjacent land uses. Compatibility can be viewed as the degree of similarity between the land uses, or in cases where the land uses differ, what measures are being taken to mitigate the potential impact of the new development upon the adjacent land uses.

FDOT occupies a block of land bordered on the west by Woodland Boulevard, Alabama Avenue on the east, Winnemissett Avenue on the north, and Volusia Avenue on the south. This section will address the land uses which are adjacent to FDOT along each of these four streets, the degree of compatibility between the proposed development and the existing land uses, and what measures FDOT is proposing to mitigate the impact of the proposed building upon the surrounding land uses.

Woodland Boulevard

Woodland Boulevard is one of the City's primary commercial corridors. The commercial uses front on Woodland and do not extend more than approximately one half block either east or west of Woodland. In the vicinity of FDOT, the land uses include the State Farm Insurance Office, the Budget Car Rental, the Dixie Lodge, the Pinkie Resale Shop, Auto Metrics Auto Repair, Tennant Printing, C&B Mobile Service Station, the Tennaco Oil Company, and ACT Inc. Clinics. Generally, these establishments have their parking immediately adjacent to Woodland Boulevard and the building at the rear of the lots.

The proposed 85' high building will be located in the southwest portion of the FDOT site. Additionally, there is proposed a new pervious parking area between the proposed building and Woodland Boulevard. The proposed parking area will utilize any of the following type systems (paver blocks, mulch, gravel, turf?) over a stabilized base. The proposed FDOT office building is basically compatible with the existing land uses along Woodland Boulevard. However, the FDOT building will be considerably taller than the adjacent land uses. In order to minimize the impact created by the proposed facility, FDOT is proposing to have a minimum 20' wide landscape buffer. This buffer area will contain one tree every 30 feet, shrubs, and sod.

Winnemissett Avenue

The Dixie Lodge is a residential treatment facility providing care for emotionally impaired adults. It is situated on the northeast corner of Woodland Boulevard and Winnemissett. The Dixie Lodge extends about half the distance between Woodland Boulevard and Alabama Avenue. The majority of the Dixie Lodge is oriented away from the FDOT property. However, there is one block of rooms which is located between the main portion of the facility and FDOT. East of the Dixie Lodge, the property is vacant.

The northwest portion of the FDOT site presently houses the Highway Patrol and Driver's License Branch. These buildings are one story in height. There is some incompatibility between the existing Dixie Lodge and the existing Highway Patrol and Driver's License Branch. However, these land uses have been existing for some time. The area of the FDOT site proposed for the multiple story building is the southwest portion of the site. There will approximately 350' from the northern most extension of the proposed building and Winnemissett Avenue (except the area directly north of the Driver's License Office and parking area). Therefore, the proposed structure will have only minimal impact upon the Dixie Lodge and other adjacent land uses along Winnemissett. Additionally, FDOT proposes to have a 20' wide landscape buffer along Winnemissett Avenue. In this buffer area, there will be one tree every 30', shrubs, and sod. The proposed buffer is proposed to reduce the incompatibility between the FDOT site and the adjacent land uses.

Alabama Avenue

At the northeast corner of the FDOT property, the parking area is currently unpaved. This area is proposed to be paved and will continue to serve as vehicle parking. Located on the southeast corner of the FDOT property there is a paved parking lot. This parking lot extends north along Alabama Avenue for about 275'. Between the two parking areas, there is a FDOT materials lab. This is a two story building, located about 20' west of the Alabama Avenue right-of-way. The City's Earl Brown Park is located along the east side of Alabama Avenue. Along Alabama Avenue, between Winnemissett Avenue and Volusia Avenue, there are two parking lots which serve Earl Brown Park. In this area of the park, there is a portion of the jogging trail, picnic shelters, a children's playground, and free play areas. There is some incompatibility between existing FDOT land uses and Earl Brown Park. The proposed paving of the parking lot will increase the degree of incompatibility. The degree of incompatibility is minimized by the fact that the proposed multiple story building will be located along Woodland and not Alabama.

In order to minimize the incompatibility between the FDOT and Earl Brown Park, FDOT proposes to utilize a type of landscape buffer along Alabama Avenue which will provide a visual screening between the FDOT site and Earl Brown Park. This buffer will contain shrubs, such as podacarpus, which will grow to a height of approximately 10'. These shrubs will be planted on 9' centers. Additionally, there will be shrubs which will have a maximum height of 4', which will be planted on 3' centers. The visual screening will extend the entire length of Alabama Avenue, between Winnemissett and Volusia. Except the area directly in front of the materials lab, which currently is landscaped. In the area currently used for unpaved parking and the materials lab, the buffer will be a minimum of 20'. However, there is only about 5' between the existing paved parking area and the ROW. In this area, FDOT is proposing to utilize a portion of the Alabama Avenue right-of-way for the visual screening. FDOT proposes to sign an agreement with the City stating that FDOT will be responsible for maintenance of this buffer area.

Volusia Avenue

Along the south side of Volusia Avenue, between Woodland Boulevard and Alabama Avenue, there is a mixture of single family residential and nonresidential land uses. On the southeast corner of Woodland Boulevard and Volusia Avenue, there is a cluster of commercial land uses including the ACT Counseling Center, Jenkins Carpentry and Construction, and the Trophy Factory. This commercial area extends about 260' east of Woodland Boulevard. The remainder of the block is developed as single family residences.

The area proposed for the new FDOT facility extends eastward approximately 260' from Woodland Boulevard and has the potential to be located within 25' of Volusia Avenue. This area will be contiguous with the existing commercial land along the south side of Volusia Avenue. A 20' wide landscape buffer with trees located on 30' centers, and shrubs is proposed for this area of Volusia Avenue.

East of the proposed building area, there is an existing paved parking lot which extends to Alabama Avenue. There is approximately 5' between the southern edge of the parking lot and the Volusia Avenue right-of-way. The parking lot is contiguous to the existing single family residences. The greatest degree of incompatibility exists between the existing single family residences and the existing and proposed land uses on the FDOT site. FDOT is proposing a visual screening type landscape buffer between the parking lot and the residences. The type of planting and spacing will be the same as is proposed along Alabama Avenue. Likewise, it may be necessary to utilize a portion of the Volusia Avenue right-of-way for this buffer. FDOT proposes to enter into an agreement with the City to ensure that FDOT maintains this buffer area located on City right-of-way.

11. UTILITIES:

Existing utilities appear to be adequate to handle FDOT proposed requirements. Southern Bell Telephone is proposing to bring a fiber optic system to the new addition.

Per City requirements, FDOT will install a fire hydrant within 150 lin.ft. of the Fire Department's connection to the stand pipe. Minimum flow for fire protection must be a 1,000 gallons per minute (GPM) for this facility.

12. SIDEWALKS:

FDOT will construct sidewalks along Winnemissett, Alabama and Volusia Avenue per the request of the Planning Board. See attached proposed site plan for location of the sidewalks. Final site plan submittal will show location, width and profile for the proposed sidewalk.

13. ATTACHMENTS:

The following should be attached to this report:

1. Key statistics, spreadsheet.
2. Proposed schedule (Design & Construction).
3. Parking analysis (Exhibit "A").
4. Tree protection work sheets.
5. Colored site plan showing existing and proposed areas.
6. Existing drainage plan.
7. Topography map.
8. Existing landscape plan.
9. Proposed landscape plan.
10. Utility plan.
11. Sidewalk plan.

Key Statistics for Planned Development District
 Florida Department of Transportation - 5th District
 719 S Woodland Blvd, DeLand, FL
 26-Aug-91

Existing - On site	Footprint (Sq Ft)	Total Gross (Sq Ft)	No. of Employees	
District Office	11,715	23,200	92	
Gas House	609	609	2	
Location/Credit Union	1,760	1,760	6	
Soil Lab	8,975	17,950	25	
Elevator/HC Restroom	1,133	4,500		
Trailers	5,280	5,280	37	
Radio Transmitter	625	625		
Total	29,472	53,299	162	
Proposed - On site				
District Office	11,715	23,200	22	
Gas House	609	609	2	
Location/Credit Union	1,760	1,760	1	
Soil Lab	8,975	17,950	25	
Elevator/HC Restroom	1,133	4,500		
Radio Transmitter	625	625		
New Addition	34,035	110,000	405	
Total	58,852	158,644	455	
Land use				
	Existing Sq ft	%	Proposed Sq ft	%
Bldg Footprint's	29,472	8.2%	58,852	16.3%
Parking - Pervious		0.0%	22,700	6.3%
Parking - Impervious.	134,175	37.2%	175,412	48.6%
Retention	20,523	5.7%	24,643	6.8%
Landscaping and				0.0%
Other - Pervious	176,942	49.0%	79,505	22.0%
Other - Impervious		0.0%		0.0%
Total	361,112	100.0%	361,112	100.0%
Total Previous	197,465	55%	126,848	35%
Total Impervious	163,647	45%	234,264	65%
Total Sq. Ft.	361,112		361,112	
Total Acres	8.29		8.29	

EXHIBIT A

Parking Analysis for

**Florida Department of Transportation
District Five Headquarters
719 South Woodland Blvd
Deland, Florida**

INTRODUCTION:

This study determines the parking needs for the new District Office building. This includes employee, visitor, and DOT vehicle parking. Since the parking is shared with the Materials and Research building located on Alabama, the needs of that building will be included.

ANALYSIS:

The number of required employee parking spaces was calculated based upon the existing parking requirements. Currently the number of employee vehicles equals 80% of the number of positions. This is because of vacancies, annual and sick leave, out of town travel, and car pooling. For design purposes 85% of the number of positions will be used for determining employee parking. The DOT vehicle parking is being set at a maximum of 30 on-site. Any additional needed parking will be provided at the Kepler Road facility.

	Dist Off	Mat & Res	Total
Positions	429	70	499
Required employee parking	365	60	425
DOT vehicle parking	20	10	30
Visitor parking	15	3	18
TOTAL	400	73	473

CONCLUSION:

Provide parking as calculated above.

AREA TREE PROTECTION REQUIREMENTS

1. Determine the square footage of the site required for tree protection. (Acreage of site x 43,560 = square footage of the site x .15 = square footage of site required for tree protection.)
2. Determine the square footage of all area within the site proposed for tree protection.
3. Calculate deficiency.
4. Apply review standards.
5. Grant permit, recommend variance, or deny permit.

.....

1. $\frac{8.29}{}$ acreage x 43,560 = $\frac{361,112}{}$ square footage of site x .15 = $\frac{54,167}{}$ area of site required for tree protection
2. $\frac{79,505}{}$ = square footage of site proposed for tree protection
3. $\frac{54,167}{}$ square footage required for tree protection $\frac{79,505}{}$ square footage proposed for tree protection = $\frac{0}{}$ square footage of deficiency of tree protection area

tpowkst

SPECIMEN TREE PROTECTION REQUIREMENT WORKSHEET

1. Count the # of specimen trees on the site.
2. Determine the minimum # of specimen trees required to be retained on the site. Refer to Section 29A-8. (# of specimen trees per acre determines minimum number of specimen trees required to be retained per acre; minimum # of specimen trees required to be retained per acre x the acreage = minimum # of specimen trees required to be retained on the site)
3. Calculate the deficiency. (minimum # of specimen trees required to be retained on the site - the actual number of specimen trees proposed to be retained on the site = the deficiency)
4. Determine the DBH of specimen trees to be removed and multiply by .5 to determine the total inches DBH of required replacement trees.
5. Calculate total of inches DBH of replacement trees.
6. Apply review standards.
7. Grant permit, recommend variance, or deny permit

.....

1. # specimen trees on site = 11
2. # of specimen trees per acre 1.3 = .38 # of specimen trees required to retained per acre (Refer to Section 29A-8)
3. .38 # of minimum specimen trees required to be retained per acre x 3.29 acreage = 7 total # of specimen trees required to be retained on site.
4. # of specimen trees proposed to be retained = 11
5. 7 # specimen trees required to be retained - 11 # specimen trees proposed to be retained = 0 deficiency of # of specimen trees
6. inches DBH of specimen trees to be removed = 0 x .5 = 0 inches (DBH) of specimen trees required for replacement

MINIMUM TREE COVERAGE REQUIREMENT WORKSHEET

Steps

1. Determine the total # of inches DBH of trees required for the site. (Acreage x 43,560 sq. ft. / 2,500 = # of trees required; # of trees required x 6" = total # DBH of trees required for the site)
2. Calculate credit of the actual # of inches DBH of specimen trees to be retained on the site.
3. Calculate credit for non-specimen trees to be retained on the site. (Credit is either actual inches DBH or assume 9" DBH for each tree.)
4. Calculate deficiency.
5. Standards of review.
6. Grant permit, recommend variance, or deny permit

.....

1. $\frac{8.29}{\text{DBH}}$ # acres x 43,560 / 2,500 = 144 # re-
quired trees
2. $\frac{144}{\text{DBH}}$ # required trees x 6" = 864 # inches
DBH required for site.
3. # inches DBH for specimen trees retained on site =
365.
4. # inches DBH for non-specimen trees retained on site
= 440.
5. Determine total credit by adding total of item 3. and
4 to determine total credit. Credit =
805 # inches DBH.
6. $\frac{864}{805}$ # inches DBH of trees required for site -
total # inches DBH credit = 59 #
inches DBH deficiency for the site.

Exhibit 2

FLOIDA

LAWTON CHILES
GOVERNOR



DEPARTMENT OF TRANSPORTATION

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SEP 04 1991

BEN G. WATTS
SECRETARY

719 S. Woodland Boulevard
DeLand, Florida 32720

August 23, 1991

Mr. Turgut Dervish, P.E.
President
Traffic Planning and Design, Inc.
385 Whopping Loop, Suite 1333
Altamonte Springs, Florida 32701

Re: City of DeLand/Traffic Impact Study Requirements

Dear Mr. Dervish:

Enclosed is our approach to analyzing the impact of the replacement building for District 5 headquarters. Please review this and advise if this is acceptable.

Sincerely,

G. M. Gilhooley, P.E.
District Traffic
Operations Engineer

GMG:w
Enclosure

cc: Mr. Gary Schindler
Mr. Ken Blakeley

CITY OF DELAND
PLANNING DEPARTMENT
APPROVED

Date: 10/21/91

Approval Expires: 12/31/92

G. M. Schindler
2-91-08-19

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Traffic Impact Analysis for

**Florida Department of Transportation
District Five Headquarters
719 South Woodland Blvd
Deland, Florida**

INTRODUCTION:

The Florida Department of Transportation will be constructing a replacement building for the District Five Headquarters. The building will be constructed at the existing site. The new building will house the existing employees in the building plus the employees currently located in four satellite offices. These are located on west SR 44, east SR 44, north US 17/92 and south US 17/92. The building is also being designed for an additional 67 positions (19% expansion).

METHODOLOGY:

Since the consolidation of the five offices into one will change trip patterns, the existing impacts were analyzed. These are shown in attachment one. The existing trips were then reassigned based upon all the employees being located in the one building. The reassigned trips were then increased by 19% to account for the additional 67 positions. The total number of employees proposed for the new building is 429.

ANALYSIS:

The traffic impacts due to the new building are shown in attachment 2 and attachment 3. The largest negative impact occurs on US 17/92 between Beresford and the building. The total ADT is increased by 72 (0.36%). The largest impact is a positive one. Between the building and SR 44 the ADT will be decreased by 85 (-0.42%).

Using ITE Trip Generation rate for Corporate Headquarters (714), the new building will generate a total trips of 940 vpd. This matches very closely the existing generation rate and parking lot useage.

CONCLUSION:

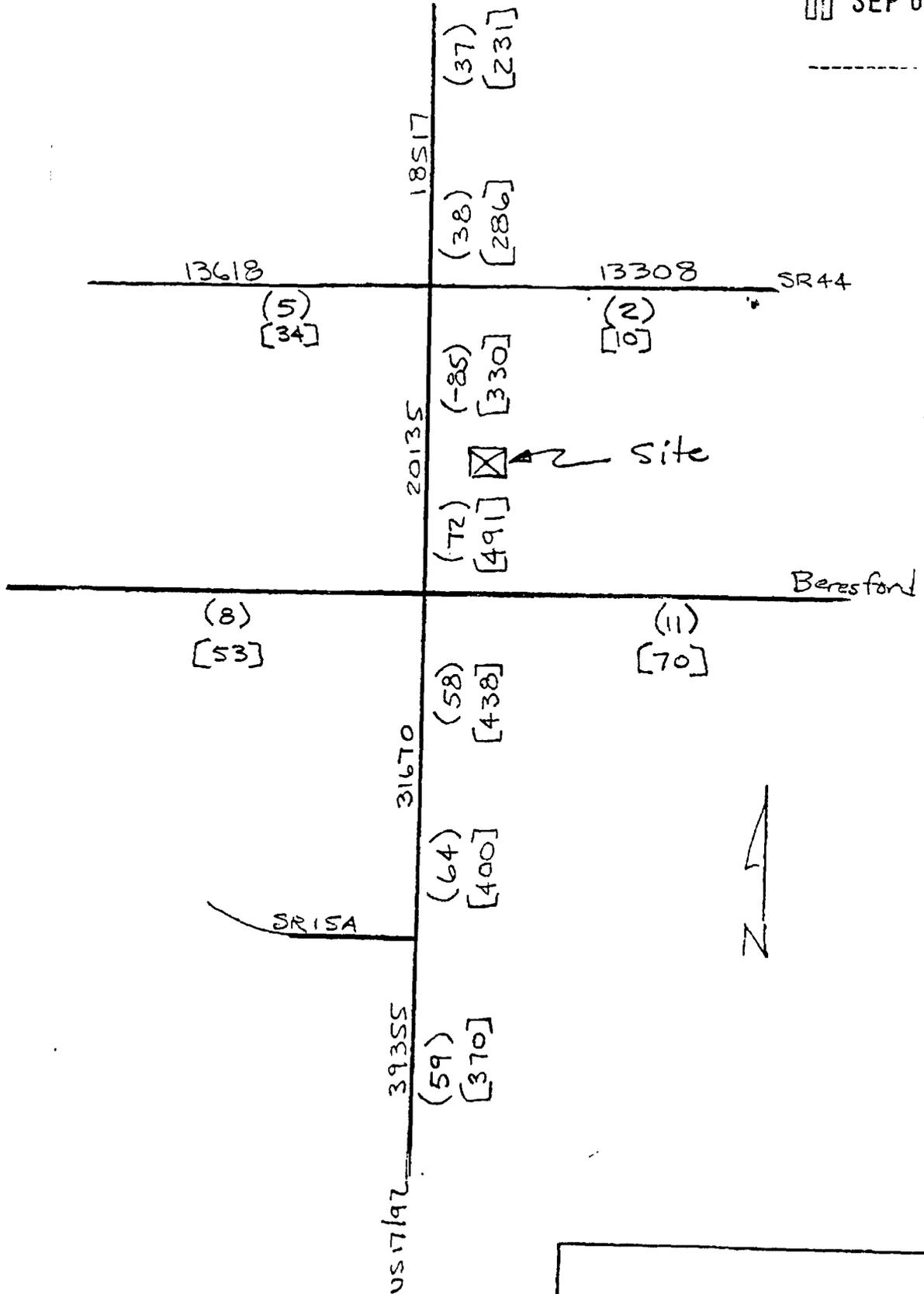
The impact of the new building is not significant with no further study needed.

**PROPOSED
CONDITIONS
(one building)**

Annex	MB south north	SR 15A to NCNB			NCNB to Beresford			Beresford to MB			MB to SR 44			SR 44 to Empire			Empire to Plymouth		
		work	home	other	work	home	other	work	home	other	work	home	other	work	home	other	work	home	other
Empire		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		24	50	0	24	52	6	24	54	10	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		14	36	0	14	36	6	14	40	6	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		6	18	2	6	18	4	6	18	6	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		0	16	2	0	16	2	0	16	4	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		0	160	8	0	160	24	0	188	27	0	0	0	0	0	0	0	0	0
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
		44	280	12	44	282	42	44	316	53	2	210	65	34	156	50	14	142	38
TOTAL		336			368			413			277			240			194		
Future (+19%)		400			438			491			330			286			231		
1989 ADT		31670			31670			20135			20135			18517			18517		
Future Additional Trips		64			58			72			-85			38			37		
% of ADT		0.20			0.18			0.36			-0.42			0.20			0.20		

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000 1989 ADT
 () Additional trips
 [] total trips

ITE Trip Generation Analysis

Corporate Headquarters Bldg (714) attached

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$$\begin{aligned} \text{Trips} &= 2.19 \times 429 \text{ employees} = 940 \\ &= 6.27 \times 100,000 = 627 \end{aligned}$$

The 2019 trips/employee better fits existing generation
therefore will use 940

Distribution (from existing analysis)

A
NORTH

↑ 33% = 310

940
trips

→ 15% = 141

↓ 52% = 489

Pos

curr	362
new	67
	<hr/>
	429

Corporate Headquarters Building (714)

**Average Vehicle Trip Ends vs: Employees
On a: Weekday**

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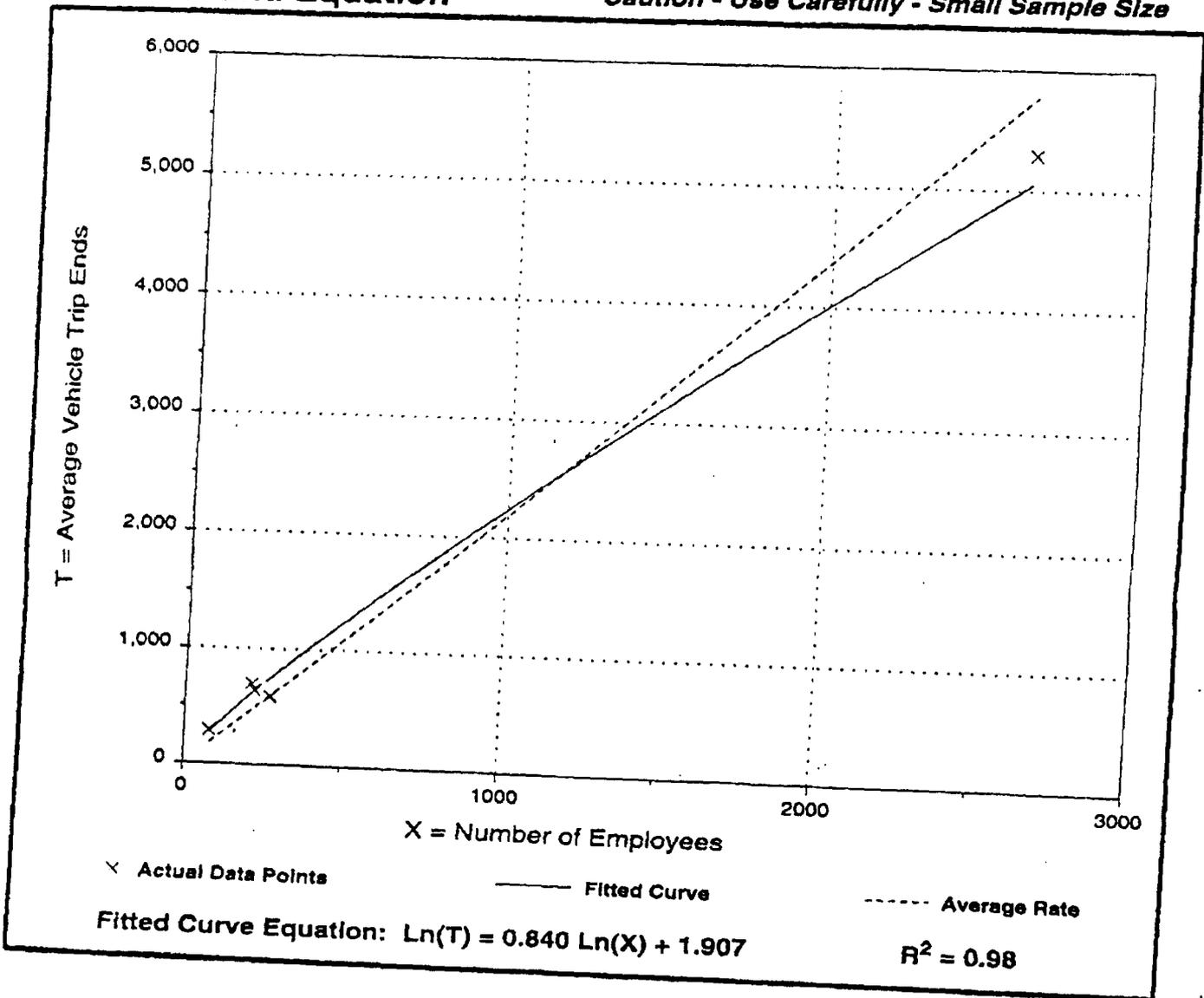
Number of Studies: 5
Average Number of Employees: 684
Directional Distribution: 50% entering, 50% exiting

Trip Generation per Employee

Average Rate	Range of Rates	Standard Deviation
2.19	2.01 - 3.52	1.54

Data Plot and Equation

Caution - Use Carefully - Small Sample Size



Corporate Headquarters Building (714)

**Average Vehicle Trip Ends vs: 1000 Sq. Feet Gross Floor Area
On a: Weekday**

Number of Studies: 5
Average 1000 Sq. Feet GFA: 239
Directional Distribution: 50% entering, 50% exiting

Trip Generation per 1000 Sq. Feet Gross Floor Area

Average Rate	Range of Rates	Standard Deviation
6.27	5.87 - 9.40	2.62

Data Plot and Equation

Caution - Use Carefully - Small Sample Size

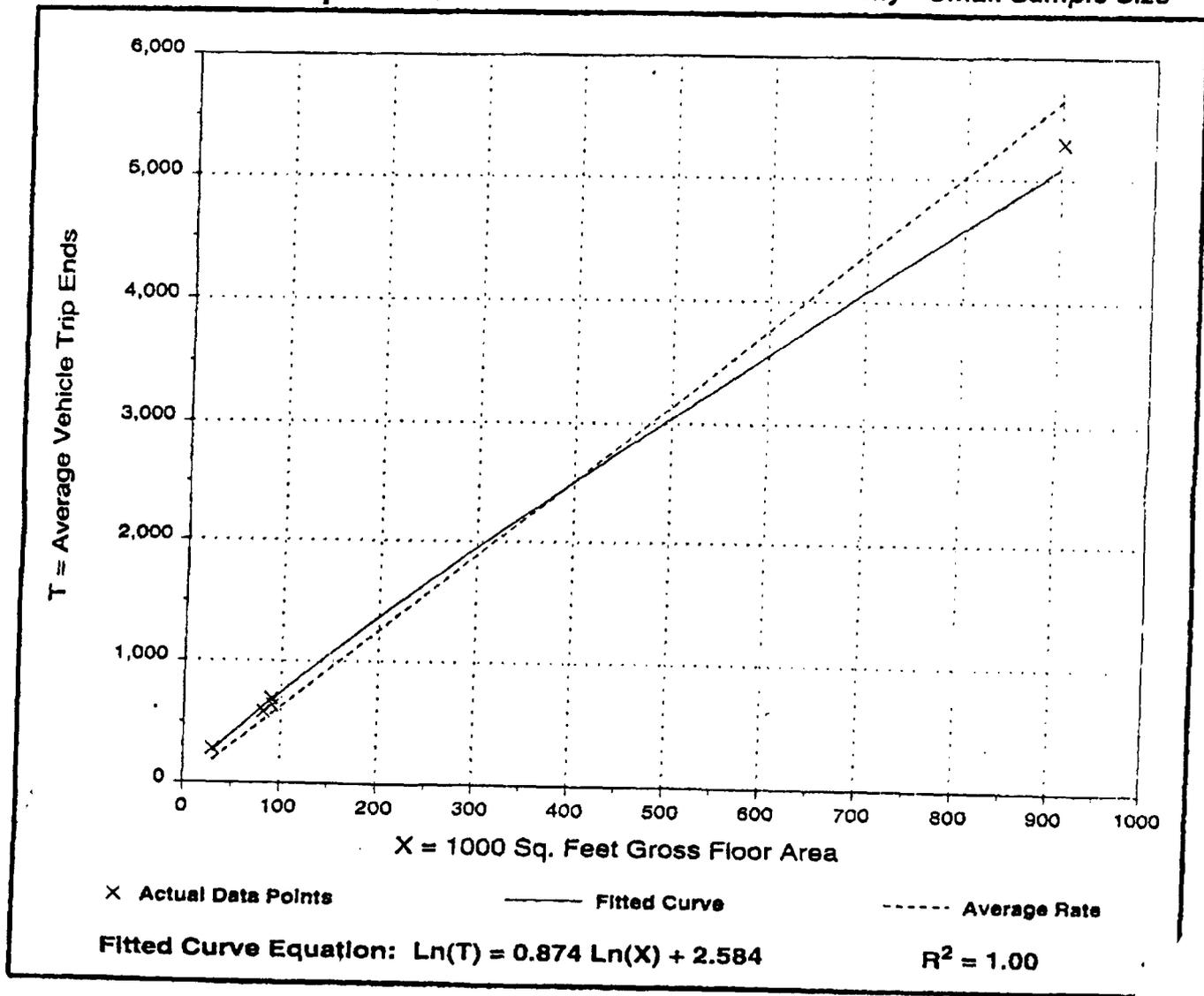


Exhibit 9

TPD

Traffic Planning and Design Inc.

385 Whooping Loop, Suite 1333 • Altamonte Springs, FL 32701 • (407) 339-5368

M E M O R A N D U M

CITY OF DELAND
PLANNING DEPARTMENT

APPROVED

TO: Gary Schindler, AICP
Planning Director

Date: 10/21/91
12/31/92

FROM: Turgut Dervish, P.E.
Traffic Engineer

Approved By: *[Signature]*

DATE: September 9, 1991

N.M. Schindler
REC'D. TR. SP- 91-08-19

RE: Florida DOT District Five Headquarters
Replacement Building

We have reviewed the traffic analysis for the new headquarters building as provided by George M. Gilhooley, P.E., District Traffic Operations Engineer. The new Florida DOT building to be constructed at the existing headquarters site will house 429 employees consisting of the existing employees at the site plus the employees in Florida DOT's satellite offices as follows:

o Employees at headquarters site.	220
o Employees at satellite offices.	142
o Additional positions.	67
Total	429

As can be seen, the number of employees at the headquarters site will increase by 209.

Trip Generation

Using ITE trip generation rate for Corporate Headquarters (Land Use Code 714), the total trip generation of the new headquarters building was estimated as follows:

- o Daily rate - 2.19 trips per employees
- o Daily generation
 $429 \times 2.19 = 940$ vehicles per day

The break down of the daily trips generated by the new building would be:

o Existing trips	482 vehicles
o Additional or new trips.	458 vehicles
	940 vehicles

As can be seen, the new trips to be generated by the Florida DOT headquarters building will be less than the 1,000 daily trip threshold established by the City for traffic impact studies.

US 17-92 Analysis

A backlogged facility, the City may permit development to occur until traffic volumes on this facility exceed a 20% increase over 1989 traffic volumes as published by the Florida DOT.

To determine whether the additional trip generation by the new building will exceed the 20% increase over 1989, a traffic assignment of the generated trips on US 17-92 was accomplished. In this assignment, the following distribution pattern was used based upon existing travel patterns of the Florida DOT employees:

o	To/from north	33%
o	To/from south	52%
o	To/from east.	<u>15%</u>
	Total	100%

Utilizing this distribution pattern, the trip generation of the new building was assigned to the adjacent roadways providing access to the site.

The critical section of US 17-92 is between Beresford Avenue and SR 44. This section had a 1989 volume of 20,135 vehicles per day. With an increase of 20% over 1989, the maximum allowable volume on this section of US 17-92 is 24,162 vehicles. The current volume on this section is 21,728 vehicles per day representing approximately 8 percent increase over 1989. Adding the new trips generated by the new Florida DOT building to this section would increase its daily volume to approximately 22,000 vehicles, well under the allowable daily volume on this facility.

Conclusions

The new Florida DOT headquarters building will not generate 1,000 or more daily trips to require a traffic impact analysis. Located on a backlogged section of US 17-92, a trip generation analysis was conducted to determine if the new building would cause the traffic volumes on US 17-92 to increase over the allowable level of 1989 volumes plus 20 percent. This analysis indicated that the maximum allowable traffic volumes of US 17-92 would not be exceeded by the construction of the new Florida DOT building.

td:js

cc: George Gilhooley, P.E.

DARK GREEN	-	Landscape Buffer
LIGHT GREEN	-	Grass Area
ORANGE	-	Existing Impervious Area - Parking
BLUE	-	Existing & Proposed Retention Areas
YELLOW	-	Existing Buildings
RED	-	Proposed Visual Barrier Area
GREY	-	Existing Sidewalks
PURPLE	-	Proposed Sidewalks
BROWN	-	Area Proposed Building Can Be Built Within
DARK ROSE	-	Proposed Impervious Parking Area In Front Of Building
LIGHT ROSE	-	Proposed Impervious Paving In New Parking Area
BLACK	-	Proposed Pervious Paving In New Parking Area